

Figure 3 – Hook Bracket/eyebolt Positioning

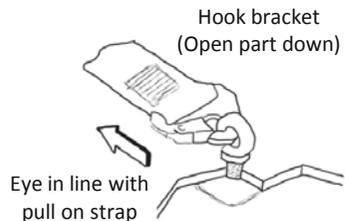


Figure 4 – Eyebolt Spacing

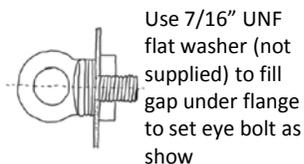
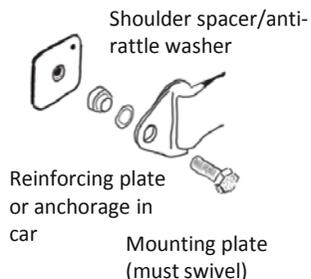


Figure 5 – Anchor Plate/Bolt Fitting



When using standard bolt fittings, assemble the end plates as shown in figure 5, making sure that the crank in the plate is away from the vehicle body work. When fitted correctly the end plates should swivel under a small amount of friction.

This guidance applies both the use of makers anchorages and new anchorages created by the installer.

We recommend that the harness strap adjusters are positioned on the breast bone.

Twin tail straps should cross over approximately 300mm (12") behind the seat back and should not be able to contact the seat or any other part of the vehicle hardware when the harness is in use.

USING THE HARNESS

Firstly the webbing straps at all shoulder and lap strap positions should be extended, this is done by tilting the adjuster at about 90° to its normal position and pulling the webbing through as shown in figure 6.

When seated fasten the harness by inserting the tongue into the slot in the buckle until a positive 'click' ensures correct engagement. Next tighten both lap straps in turn by pulling the free end of webbing up through the adjusters until the buckle and tongue are central to the wearer. Ensure that the strap passes over the bony part of the hip/pelvis and is adjusted until the straps are as tight as comfortably possible. Finally, tension the shoulder straps by pulling the loose end of webbing through each adjuster. These straps should pull firmly on the shoulders but not so tight that they start to pull the lap strap upwards.

To release the harness press the red button on the buckle as show in figure 7.

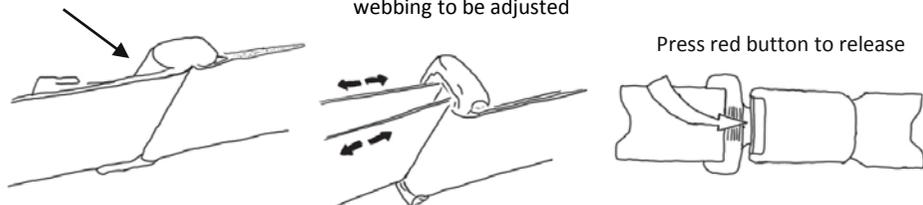
Figure 6

Figure 7

Adjuster in locked position

Raised position to allow webbing to be adjusted

Press red button to release



CLEANING

Use only a non-detergent soap to clean webbing. All other parts should be kept clear of dirt as they cannot be dismantled for cleaning. If in doubt contact the manufacturer for further advice.



SAFETY BELT SOLUTIONS
DESIGN, MANUFACTURE, TEST AND APPROVE



User Operating Instructions
For

3 & 4 point harnesses

It is important that you read these instructions carefully and completely before use. Do not modify this kit in anyway as this may affect the performance of the product. If in doubt contact Safety Belt Solutions Ltd.

Manufactured by Safety Belt Solutions Ltd in the UK



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Instructions for use

Clubman Harness For Universal Fitting

Please read instructions right through before commencing fitting. No alterations should be made to the assembly or method of fitting.

IF IN ANY DOUBT ABOUT THE USE OR SERVICEABILITY OF THE HARNESS CONSULT THE MANUFACTURER.

GENERAL

This harness fully complies with E.C.E regulation No. 16.

The correct installation, use and maintenance of the harness is paramount to its continued performance and reliability. The harness is suitable for fitting at either left or right hand front seat and will require the provision of either 3 or 4 anchorage points. If suitable 7/16" U.N.F. threaded anchorage points are already provided by the vehicle manufacturer these should be used in preference to constructing new points. In other cases the position of these anchorages may be unsuitable.

This harness is intended for use by a single adult size occupant only, in a forward facing position. Do not use for more than one person, even in the case of small children.

To satisfy legal requirements in some regions the lap and diagonal seat belt, if fitted, may need to be left in place.

IMPORTANT WARNINGS

- 1) Inspect the harness every time before use. Check condition of stitching, webbing, adjusters, anchorages and the release mechanism.
- 2) Do not under any circumstances modify this harness. Alterations may render the harness ineffective.
- 3) Webbing must never be allowed to rub against sharp surfaces on seats or bodywork or come into contact with battery acid.
- 4) Harnesses that have been cut, frayed, damaged, or used on vehicles involved in accidents must be replaced.
- 5) Do not use bleach, solvents, petrol or dye as they will affect the performance of the webbing.
- 6) Always use the harness correctly adjusted and never run a loose harness.
- 7) Do not attempt to drill any mounting holes in the vehicle bodywork before checking for wiring, pipes, tanks, double skin or unsuitable load bearing bodywork.
- 8) The use of thickly upholstered and/or a seal which may distort or collapse in an accident will reduce the performance of this harness.
- 9) Lap belt/straps which are not vertically below the hip joint will reduce the performance of the harness.
- 10) After use, ensure that the harness is stowed away neatly and no part of it trapped in a door or folding seat, etc.

If in doubt about installation, use or serviceability of this harness contact the manufacturer.

INSTALLATION REQUIREMENTS

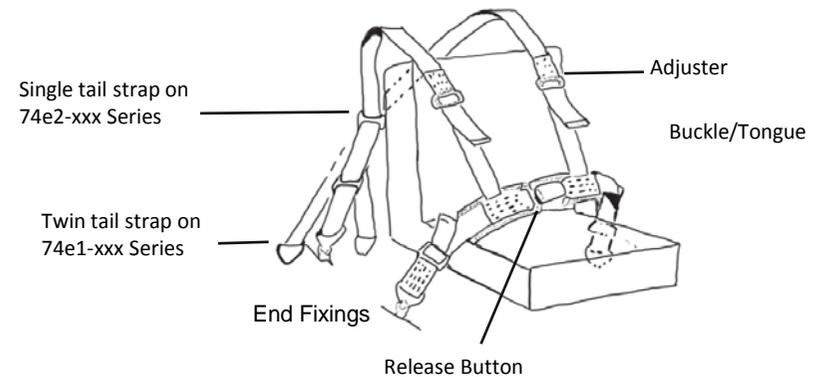
Anchorage points are required at either side of the seat and one or two at the rear for the tail straps.

Lap and shoulder anchorage points should be capable of accepting a static load of 3500lbf (1590kg). If two straps attach to the same point that point should withstand the sum of both anchor load requirements.

All mounting points should accept 7/16" U.N.F. high tensile steel threaded eyebolts.

Refer to figure 1 (General Assembly) which illustrates the typical layout of webbing and anchor points. Straps should run directly to the anchorage point with no obstruction or deviation. Straps should not be twisted and severe twists should not be induced by the installation when the harness is worn.

Figure 1 – General Assembly

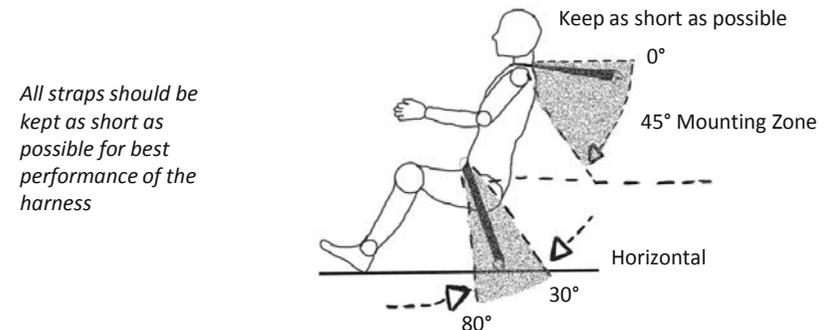


The lap belt part of the harness should not pass over the sides of the seat but through it in order to wrap and hold the pelvic region over the greatest possible area, the lap belt passing over the bony part of the hip. Under no circumstances must it be worn over the abdomen. Lap belts should terminate symmetrically about the wearer on either side of the seat about 20" (500mm) apart. The distance between the seating surface and the anchorage point should be kept to a minimum to prevent submarining.

Figure 2 shows locations for the tail straps which should be horizontal to 45° below the horizontal or as close to the shoulder as is practical for optimum restraint.

The manufacturer's seat belt anchorage's may be used where they satisfy the requirements above and are centrally positioned behind the front seat to which the harness is fitted. Twin tail strap anchorages should be set about 12" (300mm) to 20" (500mm) apart.

Figure 2 – Tail Strap Location



FITTING THE HARNESS

For lap fittings, bolt end connector plates to anchorage points as in figure 5.

For upper fixings connect the respective harness hook end brackets to the 'eyes' as in figure 3.

For upper fixings with an anchor plate see figure 5.

Positioning the eye bolt as shown in figures 3 and 4 is necessary to reduce the risk of the hook end bracket twisting off in a 'roll-over' situation and retains its maximum strength when installed. Failure to use packers under the eye bolt head could result in the loss of up to 50% of the bolts ultimate strength.